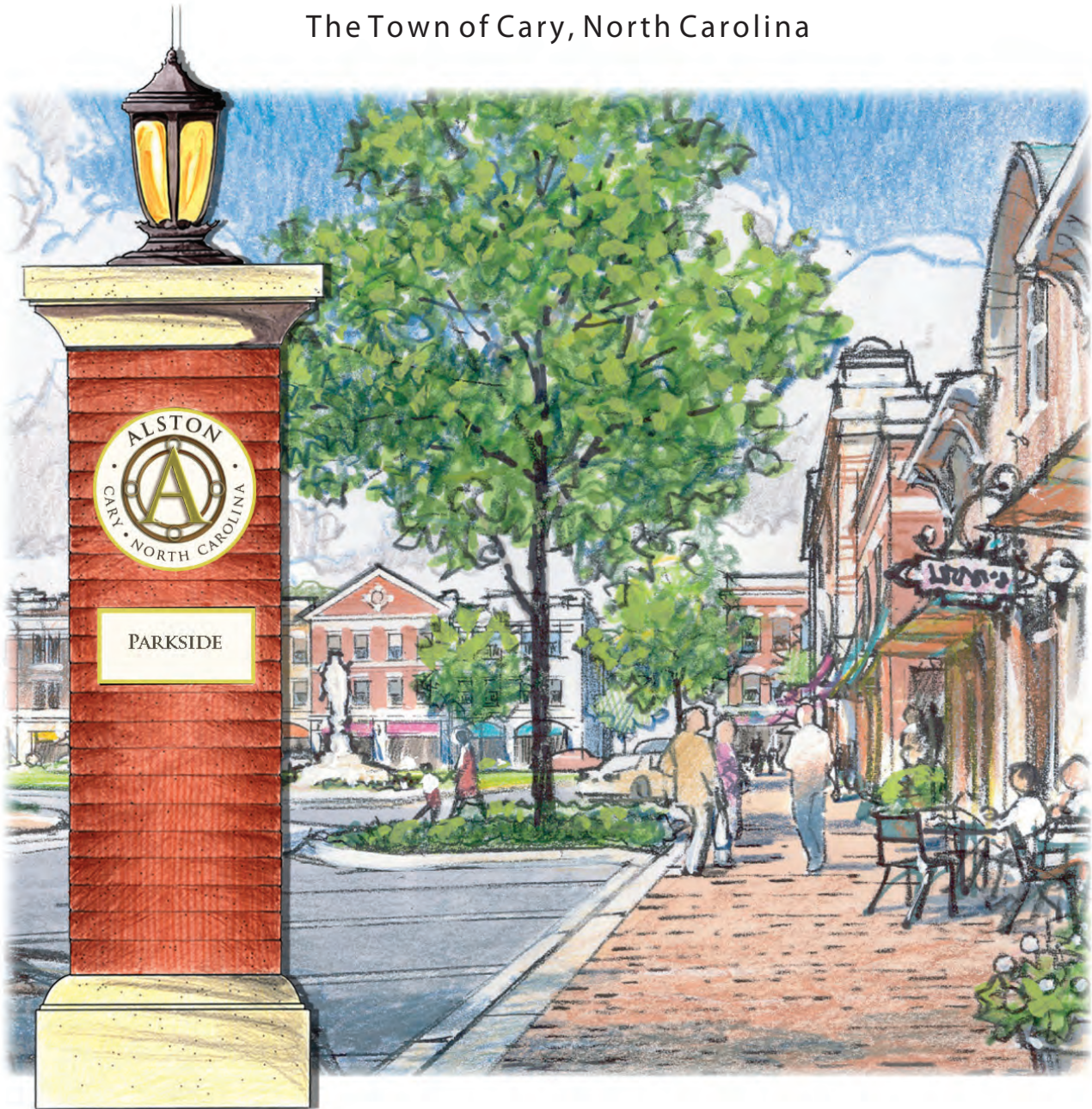


THE ALSTON ACTIVITY CENTER CONCEPT PLAN

f o r

The Town of Cary, North Carolina



prepared by

Design Collective, Inc., Architects & Town Planners

and

The Town of Cary, North Carolina

Adopted March 23, 2006
Amended December 11, 2008
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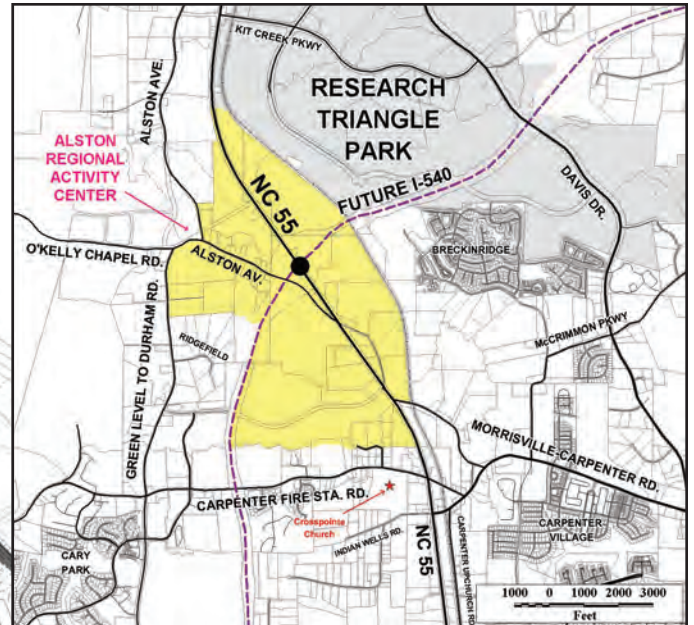
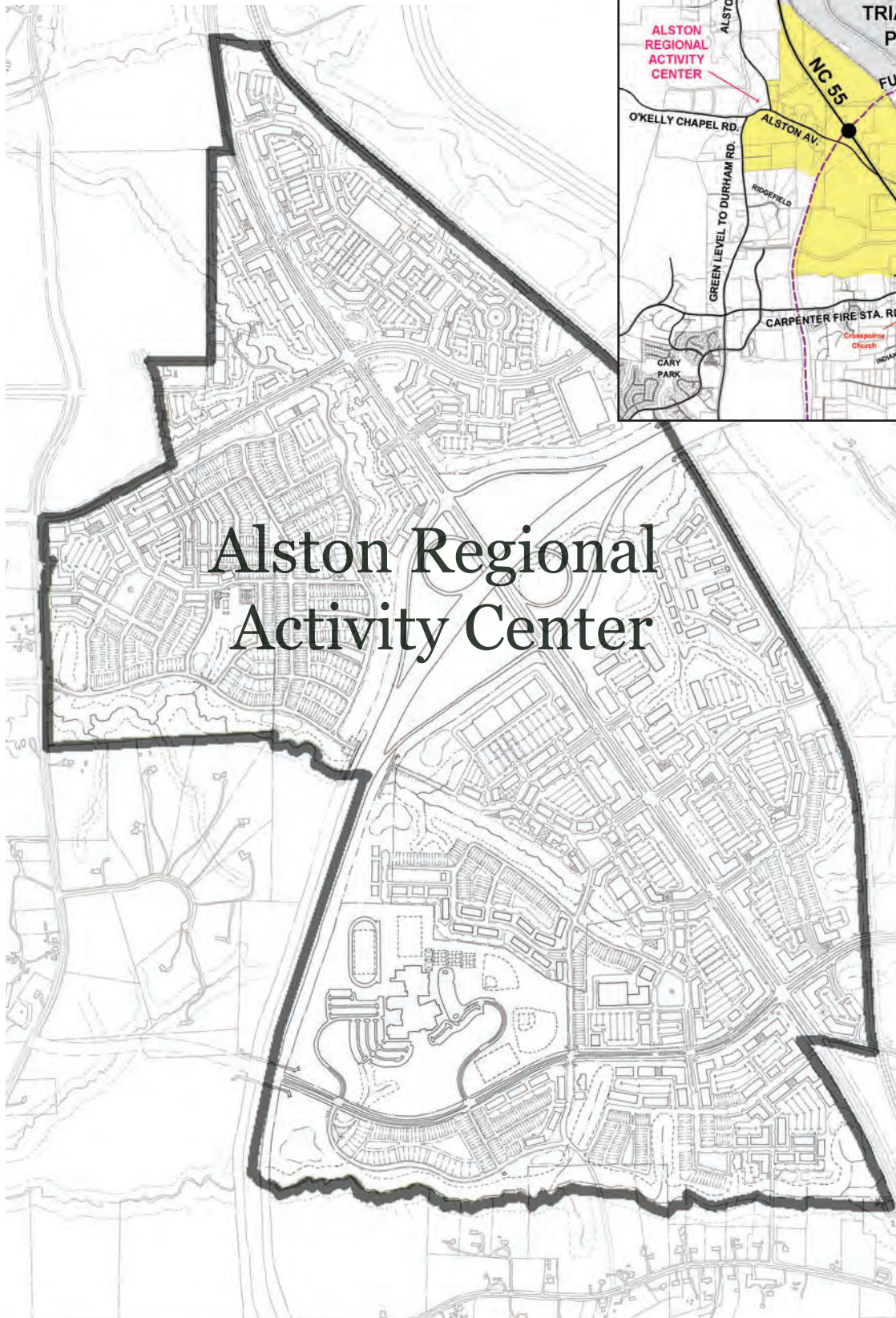
A special thanks to Crosspointe Church for providing studio and assembly space for the week-long charrette design process.

* project liaison

I - INTRODUCTION

1 - HIERARCHY OF PLAN GEOGRAPHY

BOUNDARY MAP



VICINITY MAP

The Town of Cary has prepared this Plan for the Alston Regional Activity Center (RAC), an area of approximately 970 acres at the intersection of I-540 and NC 55.

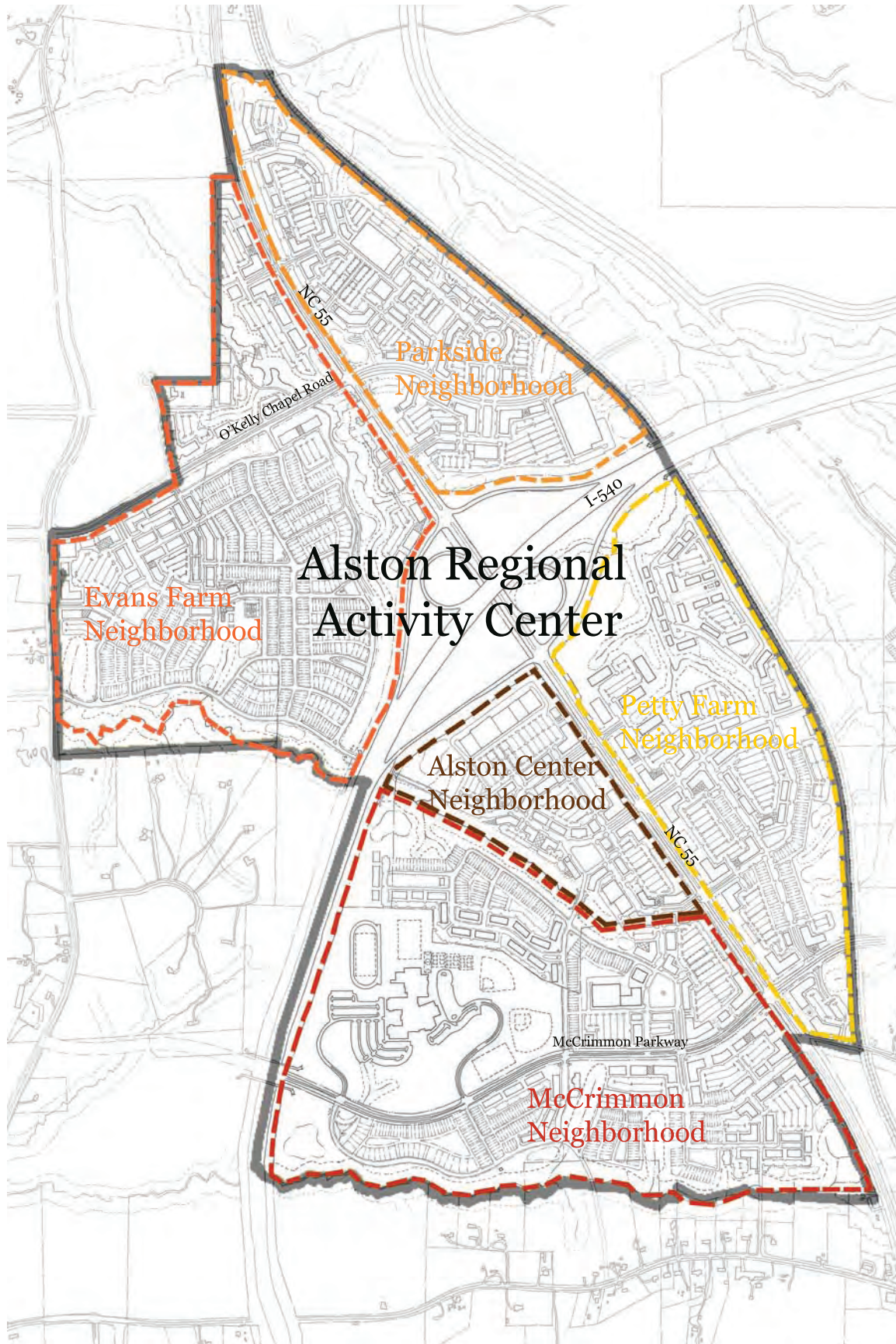
This document is divided into four chapters: I--Introduction, II--Allowed Uses and Development Limits, III--Design Standards, and IV--Appendix. The hierarchy of geography in the Alston Activity Center Concept Plan (ACCP) includes the boundary of the Alston Regional Activity Center (see Figure I.1), five neighborhoods (see Figure I.2), fifteen Traffic Analysis Zones (see Figure I.3), and five Design Zones (see Figure I.4).

The Alston ACCP will be re-evaluated by Staff and Stakeholders every two years after Council adoption. The first re-evaluation of this plan was completed in December 2008.

Figure I.1

I - INTRODUCTION

1 - HIERARCHY OF PLAN GEOGRAPHY



NEIGHBORHOOD CHARACTER

The diagram at the left illustrates the five neighborhoods within the Alston RAC: Alston Center, McCrimmon, Evans Farm, Parkside, and Petty Farm. Properties within each neighborhood will conform to, support, and create the general neighborhood character described starting on page 5 of this document.

Figure I.2

I - INTRODUCTION

1 - HIERARCHY OF PLAN GEOGRAPHY

ALLOWED USES AND DEVELOPMENT LIMITS BY TAZ (TRAFFIC ANALYSIS ZONE)

See Chapter II Part 1, where information related to allowed uses, commercial and office square footage limits, and residential unit limits is organized by TAZ. The permitted maximum amount of development allowed, per land use type, is regulated at the level of TAZs.

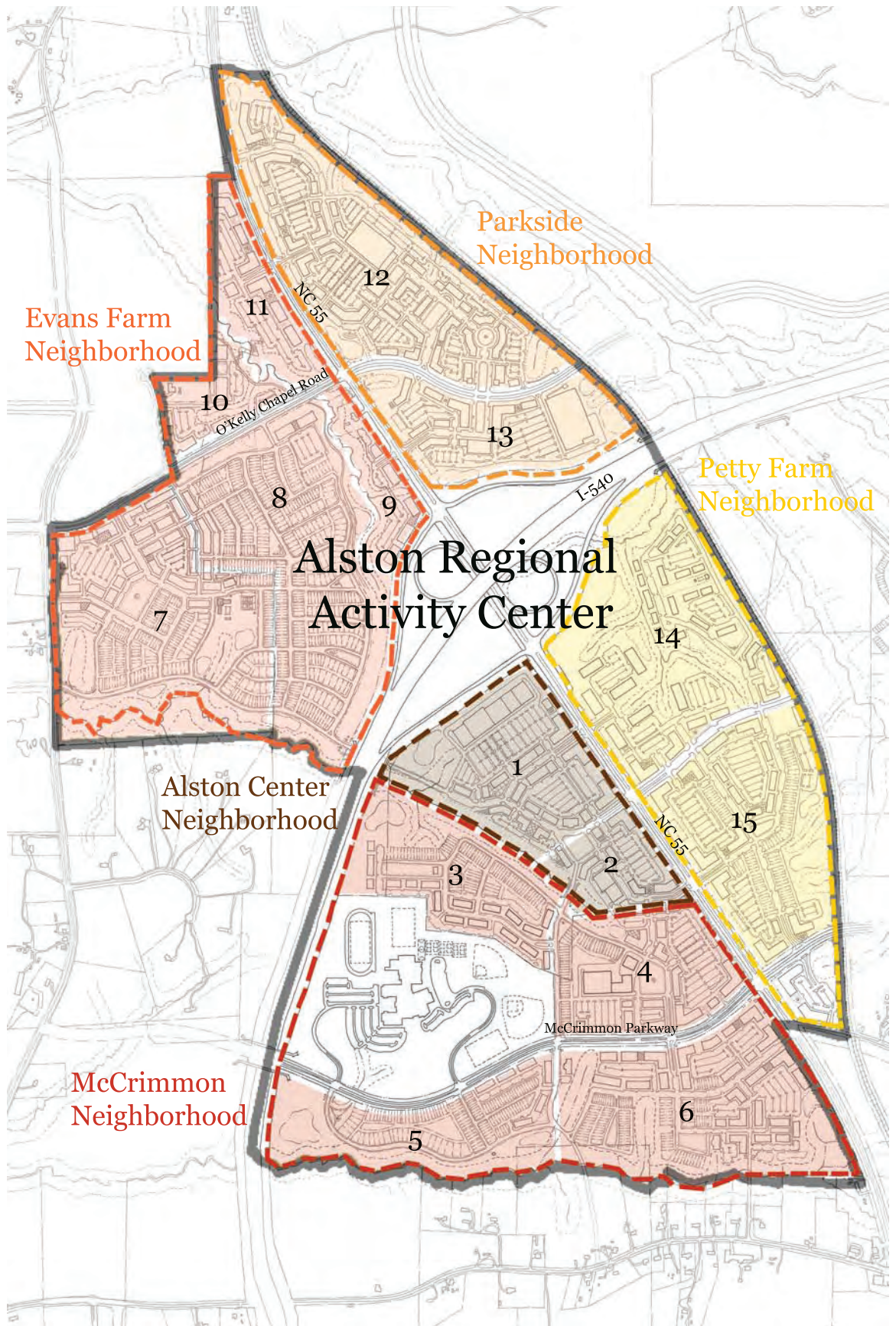
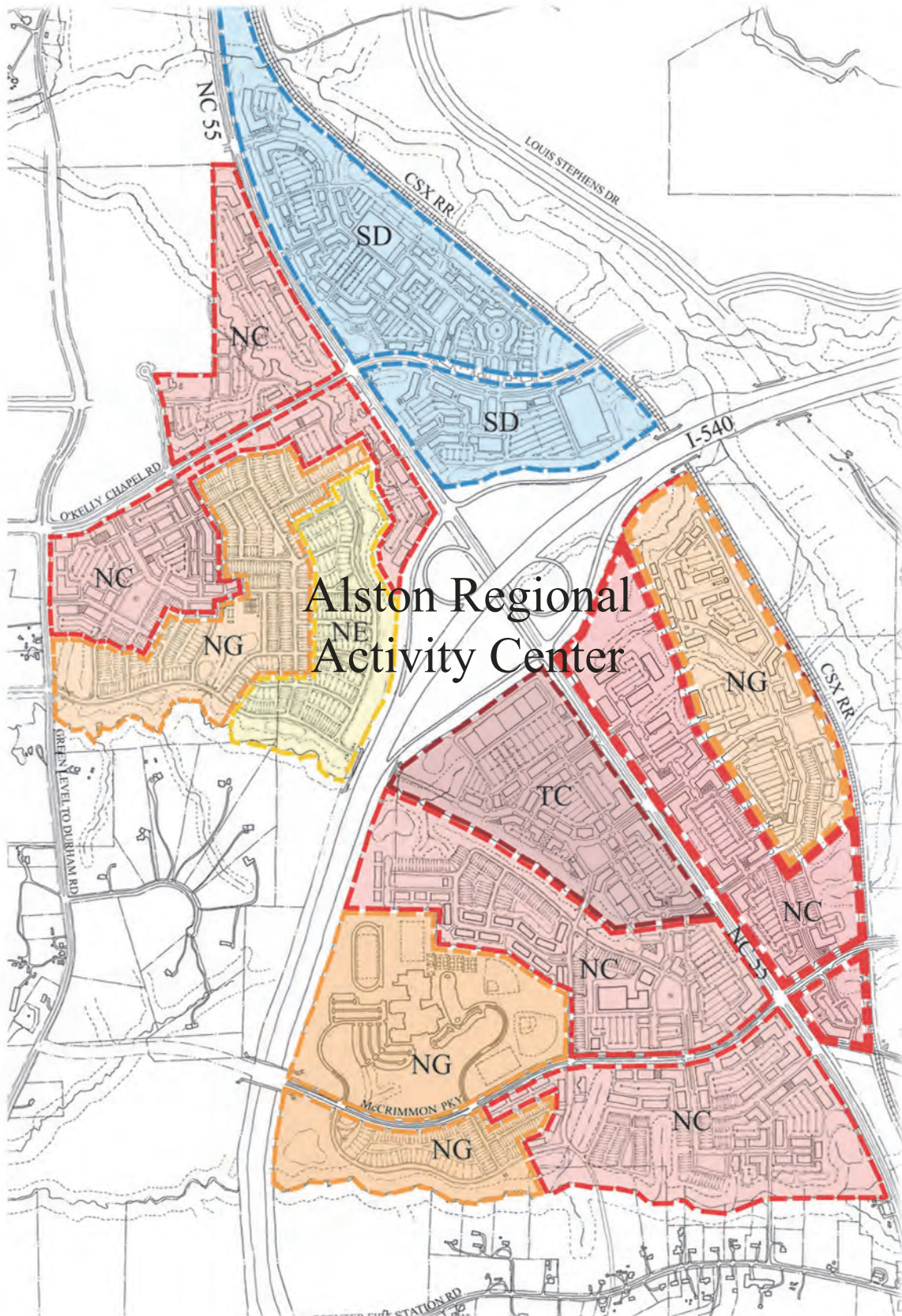


Figure I.3

I - INTRODUCTION

1 - HIERARCHY OF PLAN GEOGRAPHY



DESIGN ZONES

The diagram on the left illustrates conceptual locations for the five Design Zones within the Alston RAC: Town Center, Neighborhood Center, Neighborhood General, Neighborhood Edge, and Special District. See Chapter III Part 1, where design standards such as frontage coverage, setbacks, building height, and other provisions are organized by Design Zones.

Additional design standards for Circulation, Streetscape, and Open Space are found in the remainder of Chapter III.

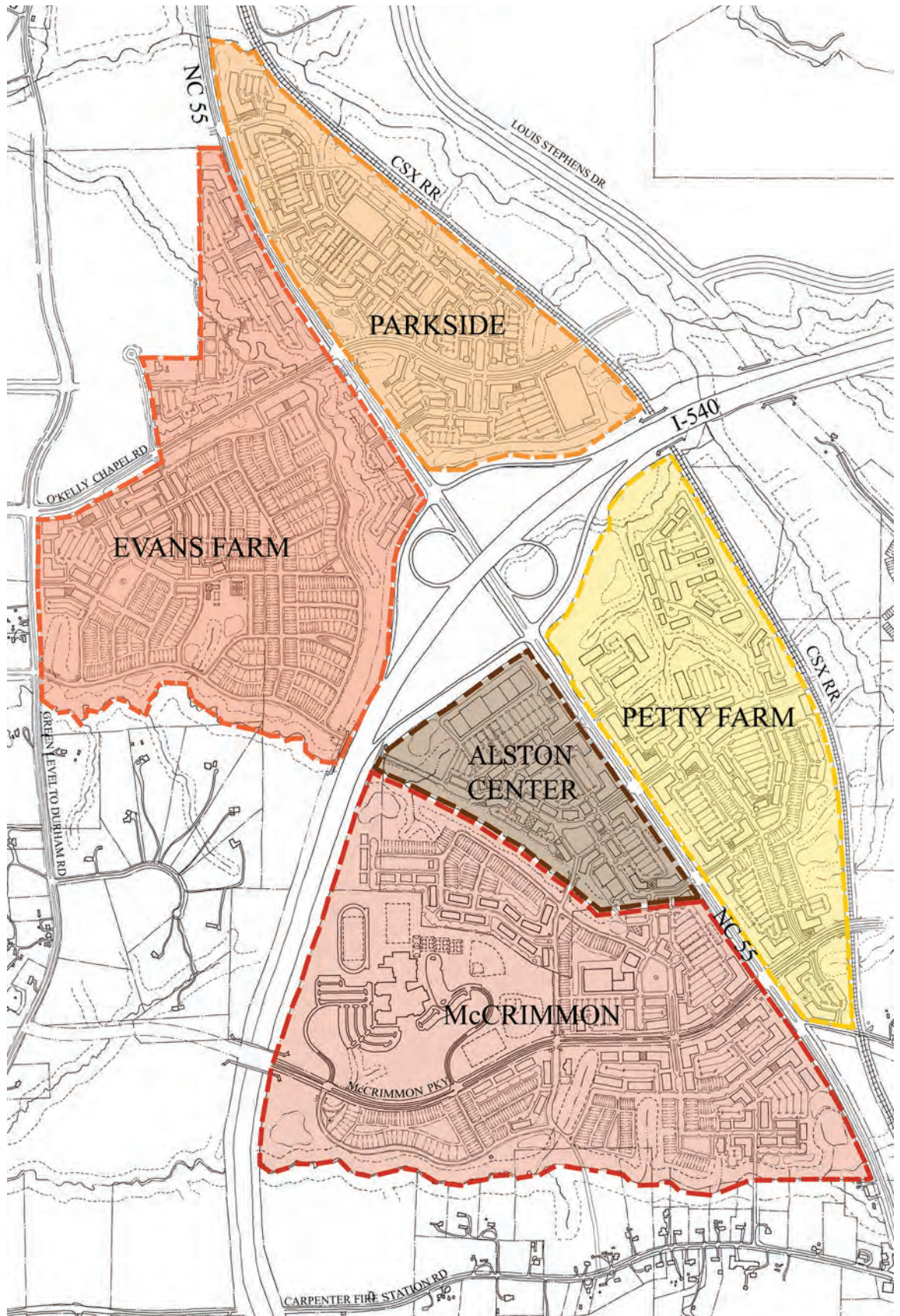
Figure I.4

I - INTRODUCTION

2 - NEIGHBORHOOD CHARACTER

The Alston ACCP contains five neighborhoods: Alston Center, McCrimmon, Evans Farm, Parkside and Petty Farm. The pattern of development within these neighborhoods should support the distinct character described below.

■ Alston Center
As the heart and hub of the new community, Alston Center will provide a destination point with a mix of retail, office, and residential uses that serve local residents. The location of Alston Center in the southwest quadrant of the plan area was determined the most advantageous due to the nature and amount of local road connections possible and the physical site characteristics. Alston Center is the most urban of the five neighborhoods and is characterized by buildings set close to the street with continuous facades, by sidewalks extending from curb to building front to allow for sidewalk cafes, and by open space in the form of plazas and squares.



1,320' = Approximately a 5 minute walk



Figure I.5

1 - INTRODUCTION

2 - NEIGHBORHOOD CHARACTER

Evans Farm

The Evans Farm Neighborhood is located in the northwest quadrant of the study area adjacent to the approved Stonewater development and the existing large-lot single-family Ridgefield subdivision. A Neighborhood Center with retail and office serving local residents may be developed within Evans Farm near the intersection of Green Level to Durham Road and O'Kelly Chapel Road. A mix of residential unit types is envisioned in the Evans Farm Neighborhood including multi-family, townhouses, and other single family homes.

Parkside

Parkside is located in the northeast quadrant of the study area. Although somewhat isolated from the other land bays of the study area, Parkside is directly adjacent to the Research Triangle Park (RTP) and is a critical bridge for employees and clients of RTP to Alston. Parkside will be a destination for RTP businesses and employees providing services which are not available within the Park including restaurants, retail establishments and hotel rooms and facilities. Residential units will be integrated into this neighborhood in the form of loft apartments (or condominium units) above commercial or in the form of hotel rooms or stand-alone higher density housing. The inclusion of these units will provide 24/7 activity.

Petty Farm

The Petty Farm Neighborhood is located in the southeast quadrant of the study area. Retail along the collector running east-west through the neighborhood provides continuity from the Town Center across NC 55. Office, light industrial and housing types including multi-family, townhomes, and live-works are envisioned in this neighborhood. Due to the proximity of the freight rail line, single family units are not currently planned. The topography and stream buffers in this neighborhood may limit building footprints.

McCrimmon

The McCrimmon Neighborhood is directly adjacent to the Alston Center and, as such, transitions between the more urban center and the residential areas south and west of Alston Center. The neighborhood contains a mix of uses including retail, office, residential and civic. The new Panther Creek High School is located within the neighborhood, and occupies approximately 70 acres. One of the goals defined in the charrette was to provide opportunities for the students to walk to sandwich or pizza shops during their lunch period. The school cafeteria will serve only a fraction of the students (300 of the total 1700 students) leaving the majority to find other options. Typically, in suburban locations, students venture off school grounds in their cars. At Alston, the choice will be presented to walk safely to shops along pedestrian-friendly streets or along the Greenway Corridors, without requiring students to cross McCrimmon Parkway.

II - ALLOWED USES AND DEVELOPMENT LIMITS

1 - USE AND LIMITS BY TRAFFIC ANALYSIS ZONE (TAZ)

TRAFFIC ANALYSIS ZONES (TAZS)

This map depicts the 15 TAZs in the Alston ACCP. The TAZ boundaries were established by the existing and planned thoroughfare and collector roadway network, physical features such as stream buffers, and ownership patterns.

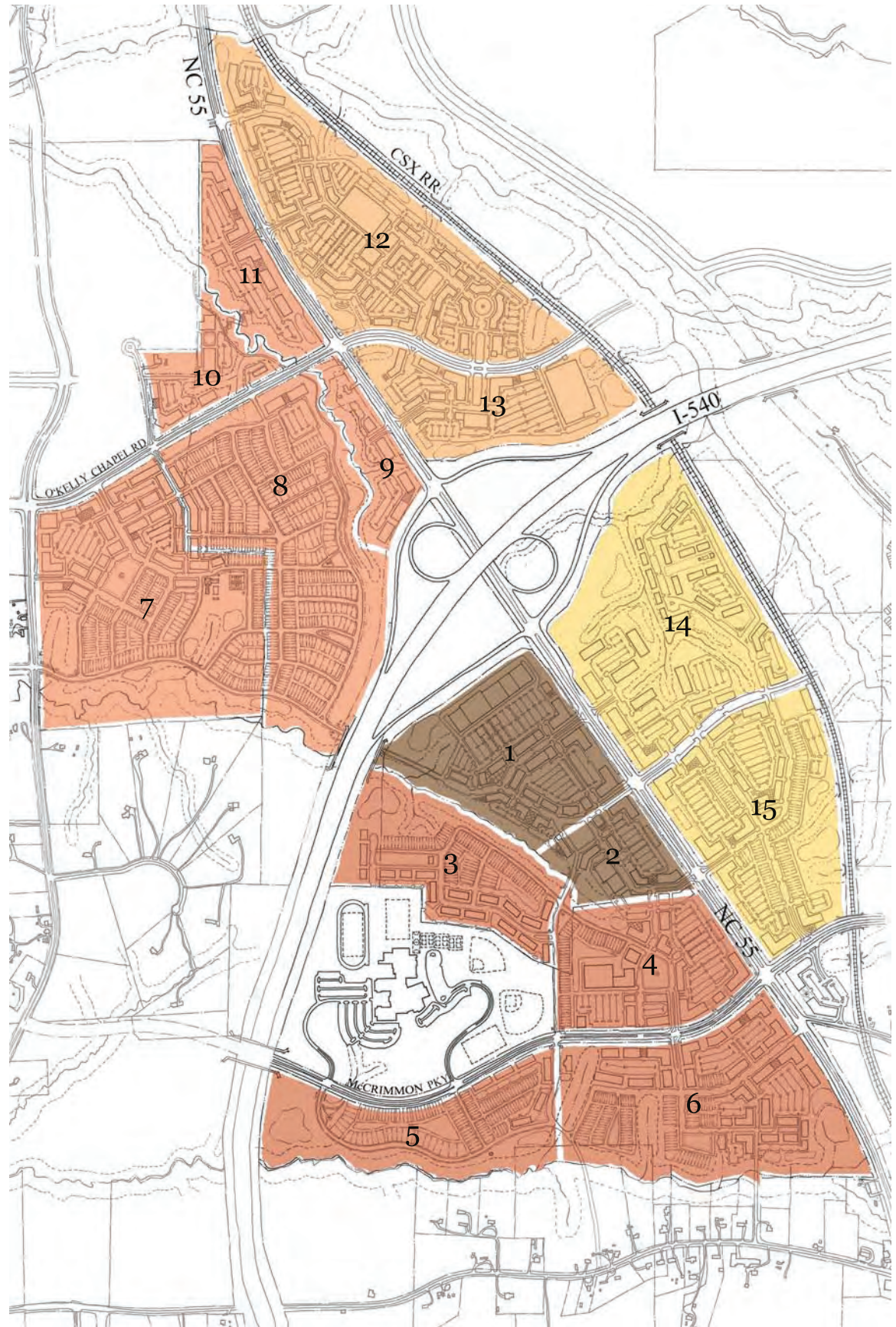


Figure II.1

II - ALLOWED USES AND DEVELOPMENT LIMITS

1 - USE AND LIMITS BY TRAFFIC ANALYSIS ZONE (TAZ)

GENERAL PROVISIONS FOR EVERY TAZ

1. BIG BOX LIMITS

No more than 50% of commercial square footage may be in big (or mid) boxes greater than 60,000 square feet (applies to footprint only). Commercial square footage in TAZs 1 and 2 and TAZs 12 and 13 may be combined to meet this requirement. The 50% cap described above may be exceeded if:

- a) no "main street" length reduction is requested, and
- b) the by-right maximum big box size is not exceeded or if structured parking is provided for any big boxes that exceed the by-right maximum so that the associated parking acreage is not increased.

2. RESIDENTIAL UNITS

Provide a minimum mix of residential types. (See allowed residential unit types for each TAZ in Table II.1. Also see design zone information in Chapter III, Design Standards, for complete information on allowed uses.)

- a) If four types are allowed, then at least two types will be built.
- b) If five types are allowed, then at least three types will be built.
- c) Hotel rooms may be substituted for one residential unit type.
- d) The minimum mix of residential unit types will be reviewed at the neighborhood level. For example, the McCrimmon Neighborhood includes TAZs 3 - 6. Collectively, there are five residential unit types allowed in these TAZs: live/work, multi-family over commercial, multi-family, single-family attached, and single-family detached. In the McCrimmon Neighborhood, there will be at least three different residential unit types.
- e) In Alston Center and Parkside, fewer residential unit types than what is listed above may be approved by the Planning Director. Residential unit types in the Alston Center Neighborhood may be applied to the residential unit mix requirement for the McCrimmon Neighborhood.
- f) If more than 50% of development, by acreage, has occurred in a neighborhood, and the required number of residential unit types are not yet present (built or approved site plan), then an additional required residential unit type must be provided in the next residential development plan. The new residential unit type must occupy at least 20% of the development site by acreage. Panther Creek High School is not included in the developed acreage calculation for the McCrimmon Neighborhood.
- g) In lieu of item f above, the layout and mix of residential unit types in the Illustrative Diagrams (Figures IV.1 - IV.5) may be followed.
- h) Residential development sites greater than 40 acres shall include at least two residential unit types. Each type may be no less than 20% of the total by acreage.

3. USING 75 - 100% OF COMMERCIAL FLOORSPACE ALLOCATION REQUIRES RESIDENTIAL UNITS

In TAZs that allow both commercial square footage and residential units:

- a) It is preferred (not required) to provide a minimum of one dwelling unit per 2000 square feet of commercial space up to the maximum number of residential units allowed per TAZ.
- b) 75% of the maximum commercial square footage allowed (see table II.1) may not be exceeded if residential units are not provided within the same TAZ. An incentive to allow increases in commercial square footage up to the maximum allowed is based on providing one residential unit for each 2,000 commercial square foot increase above 75% of the maximum commercial square footage. TAZs 1 and 2 and TAZs 12 and 13 may be combined to meet this incentive.
 - i) Residential units located in an adjacent TAZ that are within walking distance (1,350' +/-) of a TAZ's "main street" area may be counted towards meeting the residential units necessary to exceed the 75% of allowed commercial square footage. Distance will be measured following actual proposed pedestrian routes. For example, residential units in TAZ 3 that are in walking distance to the "main street" in TAZs 1 and/or 2 can provide credit for a commercial square footage increase for TAZs 1 and/or 2.
 - ii) 75% residential unit credit is given for hotel rooms. It is preferred that hotel rooms be located within walking distance (1,350' +/-) of a "main street" area.

II - ALLOWED USES AND DEVELOPMENT LIMITS

1 - USE AND LIMITS BY TRAFFIC ANALYSIS ZONE (TAZ)

TAZ Number	Maximum Commercial Square Footage	Maximum Office Square Footage	Maximum Residential Units	Allowed Residential Unit Types (see note 5 below)
<i>Alston Center Neighborhood</i>				
1	375,000 sf	120,000 sf	290	Live/work, Multi-family over commercial, Multi-family, Single family attached
2	145,000 sf	55,000 sf	50	Live/work, Multi-family over commercial, Multi-family, Single family attached
<i>McCrimmon Neighborhood</i>				
3	0 sf	0 sf	590	Multi-family, Single family attached
4	255,000 sf	85,000 sf	170	Live/work, Multi-family over commercial, Multi-family, Single family attached
5	0 sf	0 sf	460	Multi-family, Single family attached, Single family detached
6	180,000 sf	170,000 sf	440	Live/work, Multi-family over commercial, Multi-family, Single family attached
<i>Evans Farm Neighborhood</i>				
7	60,000 sf	155,000 sf	660	Live/work, Multi-family over commercial, Multi-family, Single family attached, Single family detached
8	0 sf	0 sf	670	Multi-family, Single family attached, Single family detached
9	15,000 sf	65,000 sf	0	N/A
10	10,000 sf	85,000 sf	280	Live/work, Multi-family over commercial, Multi-family, Single family attached
11	70,000 sf	125,000 sf	0	N/A
<i>Parkside Neighborhood</i>				
12	440,000 sf	345,000 sf	375	Live/work, Multi-family over commercial, Multi-family, Single family attached
13	265,000 sf	120,000 sf	0	N/A
<i>Petty Farm Neighborhood</i>				
14	50,000 sf	350,000 sf	420	Live/work, Multi-family over commercial, Multi-family, Single family attached, Single family detached
15	60,000 sf	400,000 sf	355	Live/work, Multi-family over commercial, Multi-family, Single family attached, Single family detached

Table II.1

ADDITIONAL LIMITS AND CONDITIONS:

1. In TAZs that specify a maximum amount of commercial square footage and residential units, 75% of the maximum commercial square footage allowed may not be exceeded if residential units are not provided within the same TAZ. (See item 3 on page 8.)
2. Hotel space may be taken out of either the commercial (on a square foot basis) or residential allocation (on a one hotel room equals one residential unit basis).
3. Unless there is a different agreement among property owners within a TAZ, apportioning commercial and office square footage and residential units among different property owners in one TAZ will first take into account "main street" location and length, then the location of design zones within a TAZ, and then a proportional division based on acreage.
4. Unused residential allocation in TAZ 7 can be shifted to TAZ 8, and vice versa, but any nonresidential must stay in TAZ 7.
5. Residential types, live/work and multi-family over commercial, are defined in Chapter IV Part 3 of this document. In the Allowed Residential Unit Types column on this page, multi-family includes both multi-family dwelling and boarding house. Single family attached includes both semi-detached/attached dwelling, duplex, and townhouse. Single family detached includes both detached dwelling and patio dwelling. See LDO sec-

II - ALLOWED USES AND DEVELOPMENT LIMITS

1 - USE AND LIMITS BY TRAFFIC ANALYSIS ZONE (TAZ)

tion 5.1.2 for more information on these residential types.

6. Use Table II.1 in conjunction with the appropriate design zone information in Chapter III, Design Standards, for complete information on allowed uses. All residential unit types listed in Table II.1 may not be allowed in the corresponding design zone.
7. In TAZ 15, located within the Neighborhood Center design zone, at least 75% of the area will include mixed use or non-residential development. Housing types incorporated into mixed use development may be live/work or multi-family over commercial.

USAGE ALLOCATION SHIFTING

Commercial floor space under the allocations in Table II.1 may be exchanged for an additional allocation of either office floor space or residential units according to ratios given in the table below. The table also contains ratios at which it is permissible to exchange office floor space under the allocations in Table II.1 for an additional allocation of commercial floor space or residential units.

TAZ Number	For every 1,000 square feet of unused Commercial Floorspace allocation, the Office or Residential allocations may increase by either:		For every 1,000 square feet of unused Office Floorspace allocation, the Commercial or Residential allocations may increase by either:	
	Square Feet of Office	or Residential Units	Square Feet of Commercial	or Residential Units
1	1,600	6.0	625	0.52
2	2,120	6.2	472	0.64
3	N/A	N/A	N/A	N/A
4	1,580	3.6	633	0.80
5	N/A	N/A	N/A	N/A
6	1,720	4.66	581	0.63
7	1,460	2.88	685	0.30
8	N/A	N/A	N/A	N/A
9	4,200	N/A	238	N/A
10	3,800	11.0	263	0.64
11	2,680	N/A	373	N/A
12	1,450	7.7	690	1.00
13	1,840	8.6	543	0.20
14	2,800	7.2	357	0.60
15	2,480	7.0	403	0.60

Table II.2

CRITERIA FOR SHIFTING ALLOCATIONS PER THE ABOVE TABLE:

1. Development allocation can only be shifted out of either the commercial or office usage allocation provided that a reasonable amount of that use is or will be provided within the TAZ. Thus, allocation could only be shifted out of the office category provided that a reasonable amount of office was already provided or planned within the TAZ. A minimum reasonable amount is defined as 15% of the maximum allowed square footage.
2. If office allocation is shifted to commercial uses, then for every 2,000 square feet of extra commercial space so attained, at least one dwelling unit should be provided within walking distance of the neighborhood's main street area, as described in the General Provisions for Every TAZ.
3. Subject to the preceding criteria, a maximum of up to 15% of the total allowed floorspace can be shifted out of either the commercial or office allocations using Table II.2, or up to 30% with Planning Director approval.
4. Allocation shifting between different property owners within a TAZ is permitted provided there is consent and coordinated planning among the owners.

II - ALLOWED USES AND DEVELOPMENT LIMITS

TAZ No.	<i>Exceptions to Uses Allowed for a Regional Activity Center Overlay District in the LDO</i> (These uses are not allowed in the Alston ACCP. Household Living uses are addressed in Table II.1.)	<i>Additions to Uses Requiring a Special Use Permit</i>	<i>Additions to Uses Allowed in the LDO for a Regional Activity Center Overlay District</i>
1	Group living uses, large and small home day care, Hospital uses, public athletic field, transportation facility (except transit stations), private athletic field, golf driving range, commercial outdoor amphitheater, auto sales, heavy vehicle repair, Industrial Service uses, Manufacturing and Production uses, and Warehouse and Freight Movement uses	N/A	Live/work units and Multi-family dwelling units over commercial
2	Group living uses, large and small home day care, Hospital uses, public athletic field, transportation facility (except transit stations), private athletic field, golf driving range, commercial outdoor amphitheater, auto sales, heavy vehicle repair, Industrial Service uses, Manufacturing and Production uses, and Warehouse and Freight Movement uses	N/A	Live/work units and Multi-family dwelling units over commercial
3	All non-residential uses except large and small home day care, and guest house	Large and small home day cares and produce stand	N/A
4	Public athletic field, private athletic field, golf driving range, auto sales, general industrial service, and Warehouse and Freight Movement uses, heavy vehicle repair	Large and small home day cares, produce stand, and Manufacturing and Production uses	Live/work units and Multi-family dwelling units over commercial
5	All non-residential uses except large and small home day care, and guest house	Large and small home day cares	N/A
6	Public athletic field, private athletic field, golf driving range, auto sales, general industrial service, and Warehouse and Freight Movement uses, heavy vehicle repair	Large and small home day cares, produce stand, and Manufacturing and Production uses	Live/work units and Multi-family dwelling units over commercial
7	Public athletic field, private athletic field, golf driving range, auto sales, general industrial service, and Warehouse and Freight Movement uses, heavy vehicle repair	Large and small home day cares, produce stand, and Manufacturing and Production uses	Live/work units and Multi-family dwelling units over commercial
8	All non-residential uses except large and small home day care, and guest house	Large and small home day cares	N/A
9	Large and small home day cares, public athletic field, private athletic field, golf driving range, auto sales, general industrial service, and Warehouse and Freight Movement uses	Produce stand and Manufacturing and Production uses	N/A
10	Public athletic field, private athletic field, golf driving range, auto sales, general industrial service, and Warehouse and Freight Movement uses	Large and small home day cares, produce stand, and Manufacturing and Production uses	Live/work units and Multi-family dwelling units over commercial
11	Large and small home day cares, public athletic field, private athletic field, golf driving range, auto sales, general industrial service, and Warehouse and Freight Movement uses	Produce stand and Manufacturing and Production uses	N/A
12	Large and small home day care, public athletic field, private athletic field, golf driving range, auto sales, heavy vehicle repair	Manufacturing and Production Uses and Warehouse and Freight Movement uses	Live/work units and Multi-family dwelling units over commercial
13	Large and small home day care, public athletic field, private athletic field, golf driving range, auto sales, and heavy vehicle repair	Manufacturing and Production Uses and Warehouse and Freight Movement uses	N/A
14	Public athletic field, private athletic field, golf driving range, auto sales, and general industrial service	Large and small home day cares, produce stand, Manufacturing and Production uses, and Warehouse and Freight Movement uses	Live/work units and Multi-family dwelling units over commercial
15	Public athletic field, private athletic field, golf driving range, auto sales, and general industrial service	Large and small home day cares, produce stand, Manufacturing and Production uses, and Warehouse and Freight Movement uses	Live/work units and Multi-family dwelling units over commercial

Table II.3

III - DESIGN STANDARDS

1 - DESIGN ZONES

INTRODUCTION

The Alston ACCP envisions a compact, pedestrian-oriented, mixed-use community where housing, shops, places of employment, schools, services and civic uses are integrated in a well-planned development. The Design Standards include elements to ensure this vision.

Generally, the following important design principles are inherent to these standards:

1. Blocks should be short and walkable;
2. There should be a network of streets enabling an efficient dispersion of traffic;
3. Streets should be designed for cars, bicycles, and pedestrians;
4. Buildings should continuously align all streets;
5. Parking should be to the rear of lots and buildings;
6. The types, placement, design and frequency of open spaces should be suitable to the zone;
7. There should be a variety of housing choices within each neighborhood; and
8. There should be a variety of uses, services and building types that serve the needs of residents, workers, and visitors alike.

Chapter III, Design Standards, includes five parts: Design Zones, Circulation Classification and Character, Streetscape Entries and Sections, Public Art, and Open Space and Greenways. The Design Standards specify those physical elements of the plan that collectively, through careful placement, define the physical characteristics and visual appearance of the plan and the public realm. This includes the placement of buildings and structures, the design of circulation routes, building heights and bulk regulations, streetscape, open spaces, recreation and similar elements that serve to define the public realm.

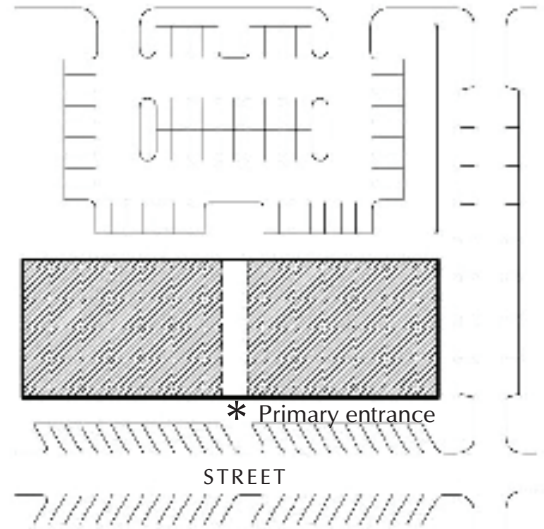
In Part I, the Design Standards for each of the five Design Zones describe how buildings and the street interface to create the character of the public realm, including such regulations as building placement, setbacks, height, and similar. Included in Part I are General Provisions for all zones as well as design standards specific to each zone.

III - DESIGN STANDARDS

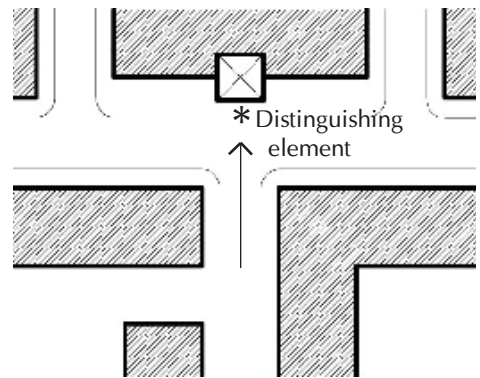
1 - DESIGN ZONES

GENERAL PROVISIONS FOR ALL DESIGN ZONES

1. Buildings along “main streets” and otherwise where feasible, including at least one entrance, shall face a street or public open space, not a parking lot. On a corner lot, the primary entrance shall face the more prominent street or public open space. Special paving details are recommended at entrances to commercial buildings. A through-lobby, open-air passage or entrance is encouraged that allows equal access from the street and from the parking lot in the rear. Storefront glazing is encouraged in open-air passages linking the parking in the rear to the street in front. [An exception to this provision is commercial buildings facing a thoroughfare. In such instances, the building shall be designed with prominent elevations and entrances on all street sides.]



2. Terminated vista: Buildings that terminate a street or important vista are encouraged to have special articulation and massing, such as a special façade, porch, bay window, tower element or similar.

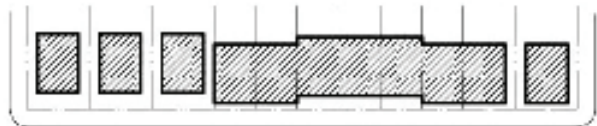


3. Variations in building setbacks shall occur in an orderly fashion. Buildings should align at the front façade. Where building setback variations occur, group the setbacks as shown in the “Preferred” and “Acceptable” diagrams. Alternating or staggering setbacks, especially for townhouses, is discouraged. Diagrams are for illustrative purposes only. There is no requirement that attached and detached buildings (residential or non-residential) need to be mixed within the same block.

PREFERRED



ACCEPTABLE



DISCOURAGED



These diagrams apply to both residential and non-residential buildings.

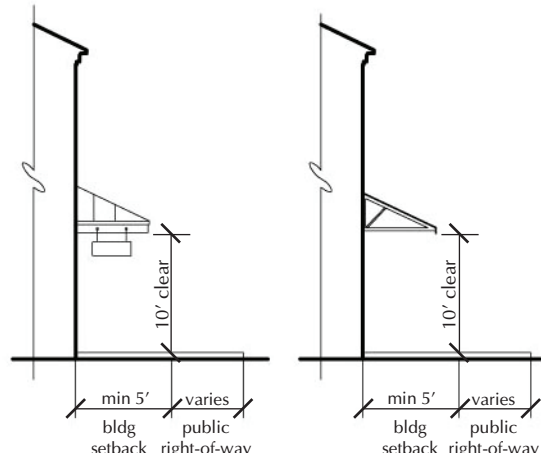
III - DESIGN STANDARDS

1 - DESIGN ZONES

4. Building Elements: Encroachments into the right-of-way shall not be allowed except for Colonnades and Arcades as described below. The following building elements may encroach into the front and/or side setbacks. Public sidewalks vary per circulation type. Public right-of-way is shown to face of curb. Depicted architecture is for illustrative purposes only to show certain setback and encroachment standards.

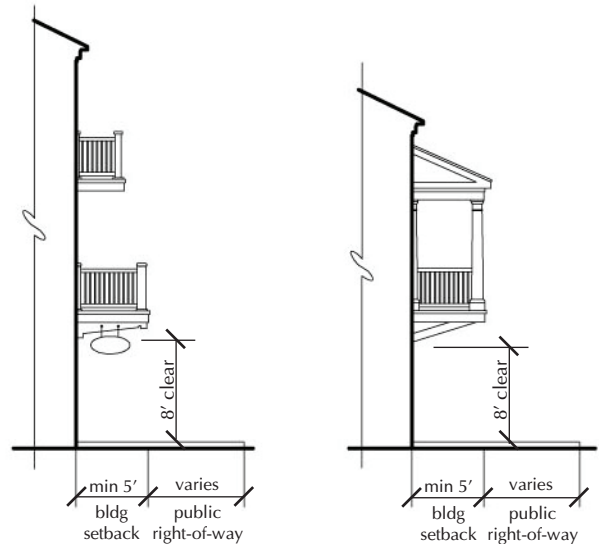
A. MARQUEES & AWNINGS:

Shall project a maximum of 8 feet forward of the façade and shall be a minimum of 10 feet clear of the grade of the sidewalk. Awning frames shall be constructed of metal and covered with fabric or metal. Fabric awnings may have side panels but shall not have bottom (soffit) panels. Metal awnings shall have no side and no bottom (soffit) panels. Awnings and marquees shall not be back lit.



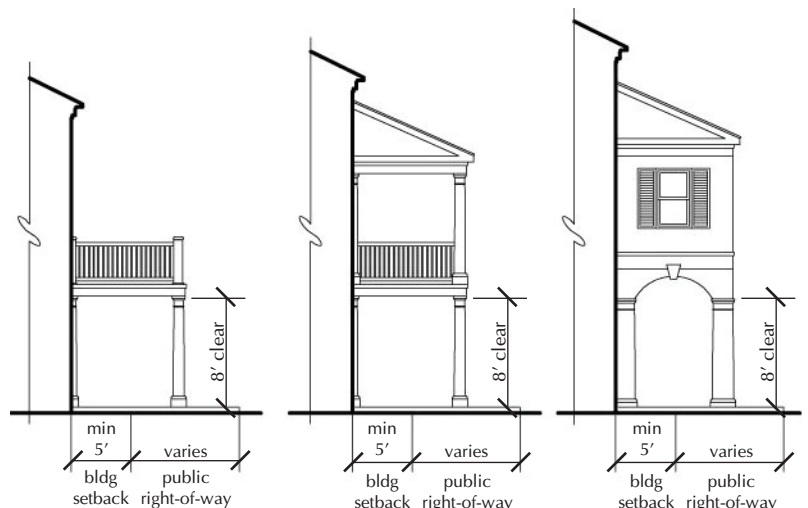
B. BALCONIES:

Shall project a maximum of 6 feet forward of the façade and shall be a minimum of 8 feet clear of the grade of the sidewalk. Balconies may have roofs, but shall be open, unconditioned parts of the building.



C. COLONNADES & ARCADES:

Shall project a minimum of 8 feet forward of the facade and shall be a minimum of 8 feet clear of the grade of the sidewalk. They shall extend the length of the facade and encompass the sidewalk/primary path of travel (*Please note: Approval of Development Review Committee, DRC, is required for any building element in a public right-of-way). Colonnades or arcades shall be a minimum of 3 feet off the face of the curb. Open multi-story verandas, awnings, balconies and enclosed, usable space shall be permitted above the colonnade or arcade.



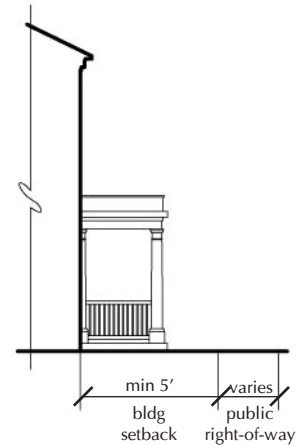
III - DESIGN STANDARDS

1 - DESIGN ZONES

Depicted architecture is for illustrative purposes only.

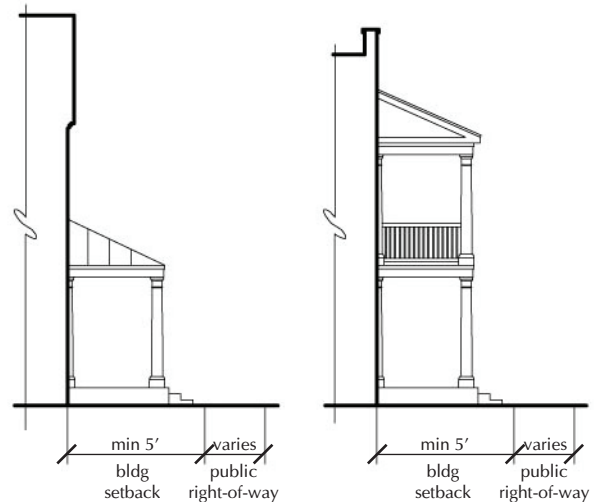
D. STOOPS:

Shall project a maximum of 8 feet and a minimum of 4 feet forward of the façade. Stoops shall be set back a minimum of 1 foot from a property line. Stoop landings shall be a minimum of 3 feet from adjacent property building elements. Stoop landing heights shall range from a minimum of 1 foot to a maximum of 4 feet above the sidewalk grade (provisions for accessible and visitable entries are allowed and encouraged, especially to the living level of a residential unit).



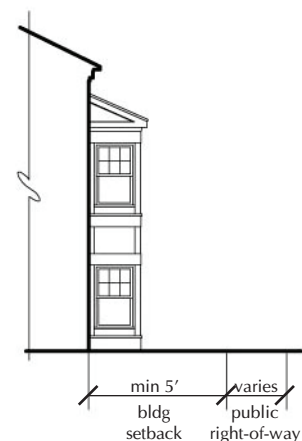
E. PORCHES:

Shall project a maximum of 14 feet and a minimum of 8 feet forward of the façade. Porches shall be set back a minimum of 1 foot from a property line. Porch landings shall be a minimum of 3 feet from adjacent property building elements. Porch landing heights shall range from a minimum of 1 foot to a maximum of 4 feet above the sidewalk grade.



F. BAY WINDOWS:

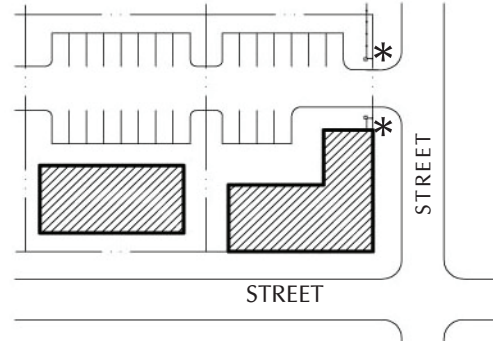
Shall project a maximum of 3 feet forward of the façade and must be a minimum of 6 feet in length across the façade. Bay windows shall be a minimum of 3 feet from adjacent property building elements. Bay windows on frontage façades shall extend to the ground or be structurally supported by brackets.



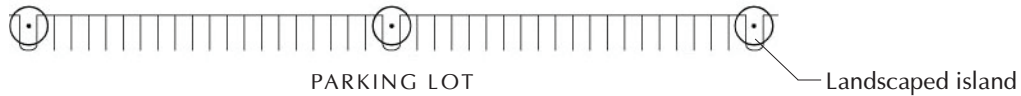
III - DESIGN STANDARDS

1 - DESIGN ZONES

- Large surface parking lots shall be placed to the rear of buildings (when possible) and/or screened from public view with a minimum 36 inch high wall and/or ever-green hedge. In order to substitute for a wall, a hedge shall be planted and maintained as a continuous element. The asterisk denotes a 36 inch high minimum wall or hedge.



- In some instances where surface parking is placed adjacent to the street, the parking lot shall be set back a minimum of 15 feet behind the front façade of the building.
- Service for small businesses and retail establishments (under 5,500 sf) may be permitted at the front door provided such service is not during primary business hours and does not disrupt traffic movement or parking.
- Parking lots shall have a minimum of one landscaped island (no less than the size of one parking space) with shade tree(s) for every twenty parking spaces. No more than twenty spaces in a row are permitted without an island.

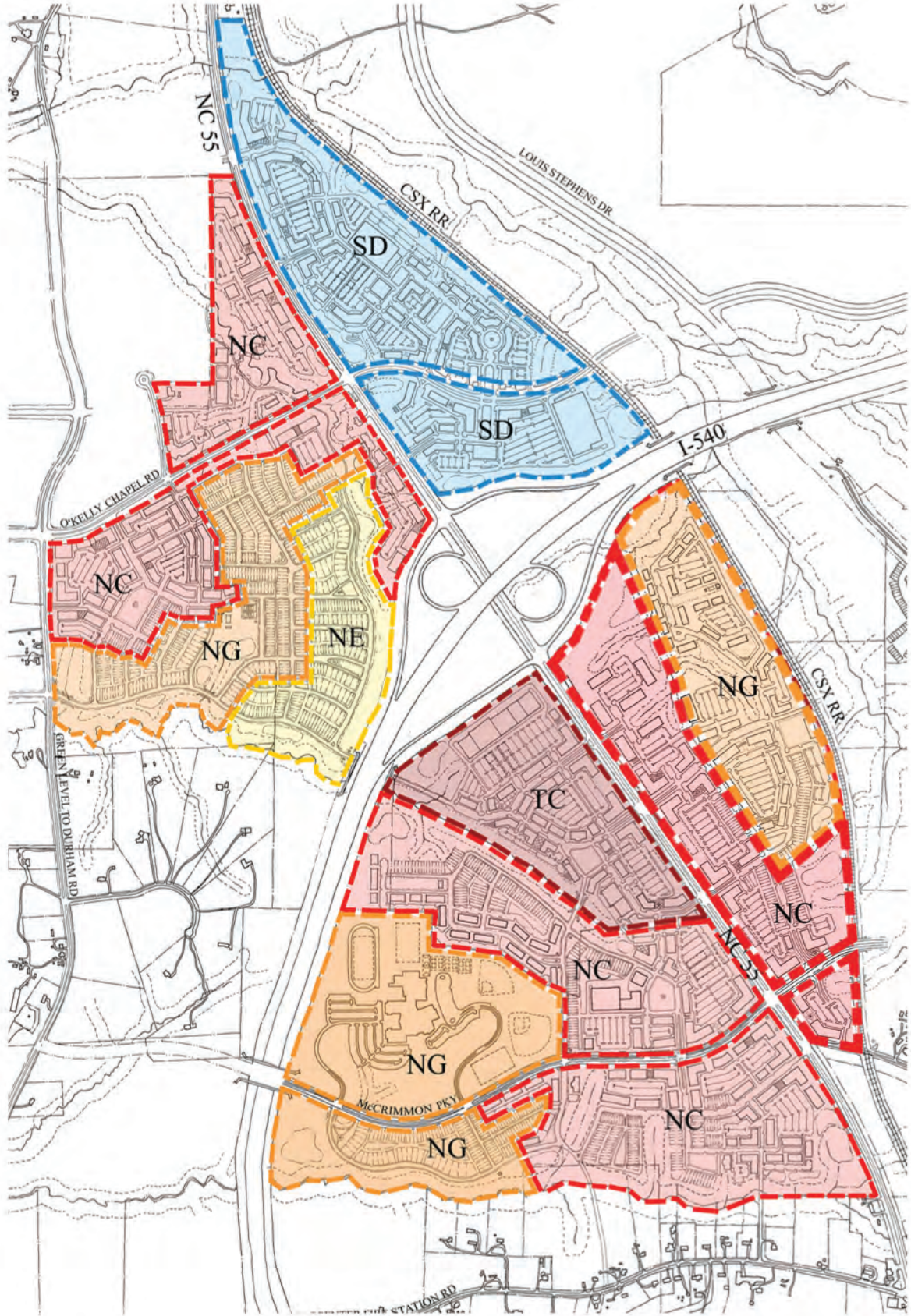


- On-street parking spaces may be counted towards the parking requirements. Reductions in parking may be granted for shared uses. (Refer to the LDO for parking requirements and shared parking provisions.)
- Where possible, garages shall be accessed from an alley or from the less prominent circulation route. Garages, and especially garage doors, shall be placed to minimize visual impact from the public view.
- BMPs (Best Management Practices), where visible from “main streets”, public spaces, and/or public right-of-ways including passages and greenway corridors, shall be designed as public amenities to resemble natural water features without steep slopes, chain link fences or similar. A hardscape edge on a BMP is permitted as long as it blends well with the project; a hardscape edge completely surrounding a BMP will require approval of the Engineering Department.
- Multifamily Over Commercial in the Alston ACCP shall be exempt from the Town of Cary LDO Section 8.2.2 (D), Requirements for Private Open Space.
- In the event of a conflict between “main street” design standards and the Town’s Design Guidelines Manual with respect to buildings fronting thoroughfares, fulfilling “main street” requirements is the priority.
- If no other provision is specified in this document, then Design Standard variations of up to 10% may be approved by the Planning Director and up to 25% by Town Council.
- Life Care Communities are allowed in all Design Zones.

III - DESIGN STANDARDS

1 - DESIGN ZONES

DESIGN ZONE PLAN



1,320' = Approximately a 5 minute walk



Figure III.1

III - DESIGN STANDARDS

1 - DESIGN ZONES

The Alston Regional Activity Center is divided into five urban design zones: Town Center, Neighborhood Center, Neighborhood General, Neighborhood Edge and Special District. The Design Standards, such as frontage coverage, setbacks, building height, open space requirements, etc., vary according to zone, encouraging higher densities and a greater mix of uses in the Town Center and Special District while appropriately transitioning to housing and less density at the Neighborhood Edge.

- **Town Center:** The Town Center is the most urban of the five zones and will be the heart of the new community. Therefore, the character shall be defined by a mix of uses with buildings close to the street, sidewalk pavement from curb to building and outdoor areas for sidewalk cafes and for community-wide gathering. This design zone must be used within the Alston Center Neighborhood.
- **Neighborhood Center:** The Neighborhood Center is a smaller center of neighborhood retail, office and residential and/or a transition between the Town Center zone and the Neighborhood General and Neighborhood Edge zones. This design zone includes commercial, office, and residential (multi-family and single-family attached mixed with commercial and office or in proximity to commercial and office) outside of the Town Center and Special District zones.
- **Neighborhood General:** The Neighborhood General is a residential zone that provides a transition from the mixed-use centers of the Neighborhood Center zone to residential as it meets the Neighborhood Edge or other natural features. This design zone includes attached and detached single-family housing and must be used between the Neighborhood Center zone and the Neighborhood Edge zone.
- **Neighborhood Edge:** The Neighborhood Edge is a lower-density residential zone with larger front and side yards than the Neighborhood General zone. This design zone must be used adjacent to the Neighborhood General zone and will include attached and detached single-family housing.
- **Special District:** The Special District establishes a unique zone for an area of the Alston ACCP that is located adjacent to Research Triangle Park (RTP). Although this area is somewhat separated from the remainder of the Alston ACCP by the major infrastructure of I-540, the widened NC 55 and the CSX rail line, the connection to RTP is vital. This area is seen as a destination for RTP employees and clients as well as having a regional and local draw. The character of this zone shall be defined by its integrated mix of uses, streetscape and pedestrian-oriented open spaces in the form of plazas and greens. This design zone must be used within the Parkside Neighborhood.

Design Zone boundaries, as shown in the map on the previous page, are conceptual. As long as the appropriate transition is provided (for example, Neighborhood Center provides a transition between Town Center and Neighborhood General) and each design zone is represented approximately as shown in Design Zone Plan, design zone boundary lines may vary/shift as needed. Use the descriptions above along with the Design Zone Plan on the previous page to determine which design zone(s) provisions to follow.

This system of classifying zones enables an appropriate and consistent urban environment to be maintained within cohesive neighborhood areas; this helps support a range of activities within each zone of a neighborhood. In this way, shopping, employment, services, recreation, schools and similar needs are provided for within the neighborhood, often within walking distance, while still being accessible to users outside the neighborhood. A variety of housing types are integrated within each neighborhood allowing some housing to be within very close proximity of uses and services while allowing lower density housing to be more private.

The Design Standards include General Provisions for all zones and provisions specific to each zone. The Town Center, Neighborhood Center, and Special District zones include provisions for main streets, frontage coverage, and non-first floor space. All of the design zones include provisions for build-to-lines, building separations, building height, and other provisions.

III - DESIGN STANDARDS

1 - DESIGN ZONES

Mixed Use

Mixed Use buildings have more than one use, often residential or office above retail.



Commercial

A single use building in which goods are exchanged or sold.



Office

A single use building in which services are transacted but goods are not exchanged or sold as the primary purpose of the business(es).



Live-Work

Any single-family, townhouse or similar residential building type in which the first floor is utilized as a compatible commercial space.



Multifamily

A residential building type housing two or more units.



Townhouse

A single family residential unit that shares at least one adjoining sidewall with an adjacent unit and is typically at least 2 stories.



Single Family Detached

A single family residential building type that does not share any walls with adjacent units.



TYPICAL BUILDING TYPES

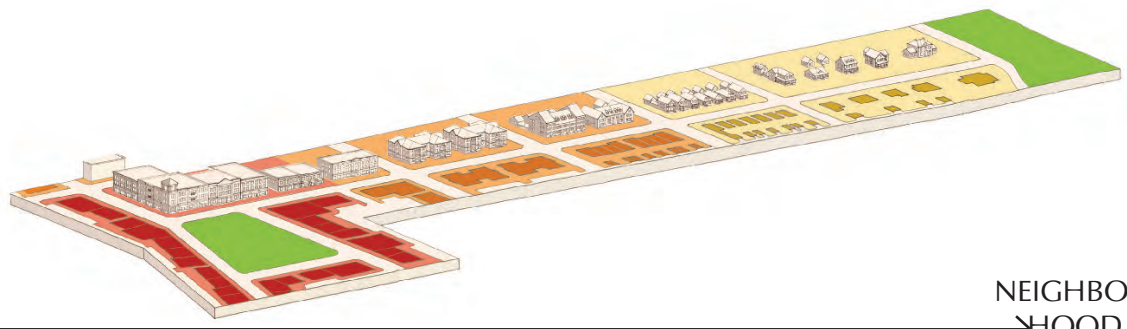
Mixed use, commercial, office, live-work, multi-family, and townhomes are typical building types found in the Town Center, Neighborhood Center, and Special District. Townhomes and single family detached homes are typical building types found in Neighborhood General and Neighborhood Edge.

TOWN CENTER AND SPECIAL DISTRICT

NEIGHBORHOOD CENTER

NEIGHBORHOOD GENERAL

NEIGHBORHOOD EDGE

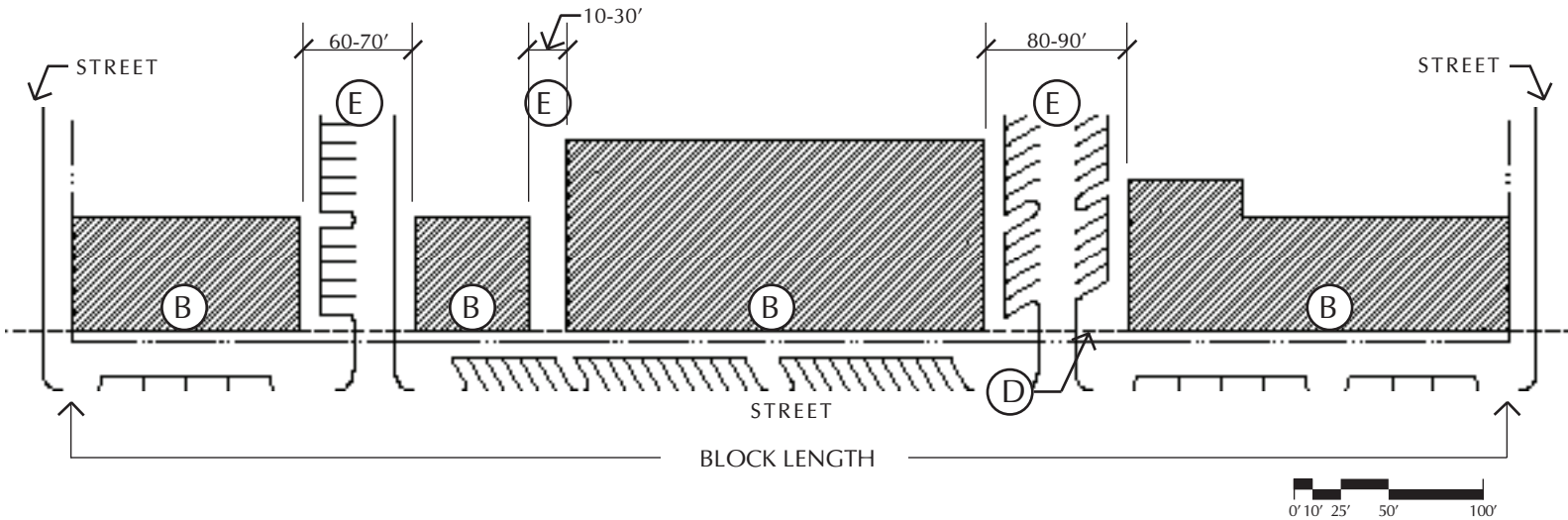


TOWN CENTER and SPECIAL DISTRICT ←
More Urban

NEIGHBORHOOD EDGE →

III - DESIGN STANDARDS

1 - DESIGN ZONES



TOWN CENTER

A MAIN STREET:

“Main streets” are typically composed of Collector and/or Street A circulation types; they meet or exceed frontage coverage and non-first floor space and vertical mixed use requirements described below. “Main streets” are consistently double-loaded except where natural features such as stream buffers or steep slopes (greater than 15%) result in a single-loaded street segment. These streets are destination points within the Alston area that foster a positive pedestrian experience; each neighborhood will have at least one contiguous “main street”.

A contiguous “main street” should be provided that is at least equivalent in length to either

- 75% of the total combined length of the required Collector and Street A segments as indicated in the Circulation Diagram (see Chapter III Part 2) but not less than 1,800 feet (the length of any Collector or Street A that must be single-loaded due to environmental or road geometry constraints counts 50% toward the required minimum length of the contiguous “main street”), or
- 2,400 feet.

Notes:

- No more than 10% of a “main street” may be single-loaded; clear hardship, or no practical alternative, must be demonstrated to include a single-loaded section. The length of any single-loaded portions of the “main street” counts 50% towards meeting the required minimum length.
- “Main street” lengths required to be 2,400 feet may be reduced to no less than 1,800 feet by Town Council.
- Up to 50% of a “main street” may be composed of a completely residential product.
- “Main streets” may be one continuous linear street or may be a collection of contiguous streets.

B FRONTAGE COVERAGE:

Frontage is defined as the linear dimension adjacent to a street along which the front façade(s) of structure(s) are placed. Frontage coverage is defined as the ratio of the total building frontage lining a street block to the total length of the block.

Frontage coverage of 70% minimum of block length (sum of building lengths divided by block length) is required on “main streets”. Block length is measured between streets (not including alleys) or to a property line or stream buffer. Required end-of-block setbacks may be subtracted from the block length.

Notes:

- Frontage coverage is achieved with buildings. However, Plazas and Squares, as specified for the Town Center zone, also count towards frontage coverage, as long as they are addressed with buildings on all sides whenever possible.

III - DESIGN STANDARDS

1 - DESIGN ZONES

- b) Area of Frontage Coverage should be contiguous and compact.
- c) Although Frontage Coverage is not required for all Circulation Types, views into large parking lots should be screened with buildings, walls, plantings, or similar.

C MIXED USE:

At least two different uses, mixed vertically or horizontally, are required along a main street. The floor space for one use may not exceed 80% of the total square footage along that main street.

D BUILD-TO-LINE (FRONT SETBACK):

Five to ten feet from property line or from private street back of curb; twenty-five to thirty feet from property line along thoroughfares. Exceptions for drive-throughs may be granted by the Planning Director.

E BUILDING SEPARATIONS/SIDE SETBACKS:

Shall be within the dimensions as noted in the diagram above. When necessary to accommodate required and/or reasonably distributed parking and/or deal with other site constraints, greater building separations/side setbacks may be allowed as follows:

- a) For every 1000 feet of "main street" length or other street length, there may be two side setback/building separations between 91 - 170 feet.
- b) Additional side setbacks/building separations between 91 - 170 feet may be approved by the Planning Director.
- c) A wall and/or landscaping will be required to fill in any building separation gaps.

F BUILDING HEIGHT:

Minimum 20 feet (or 1.5 to 2 story appearance)

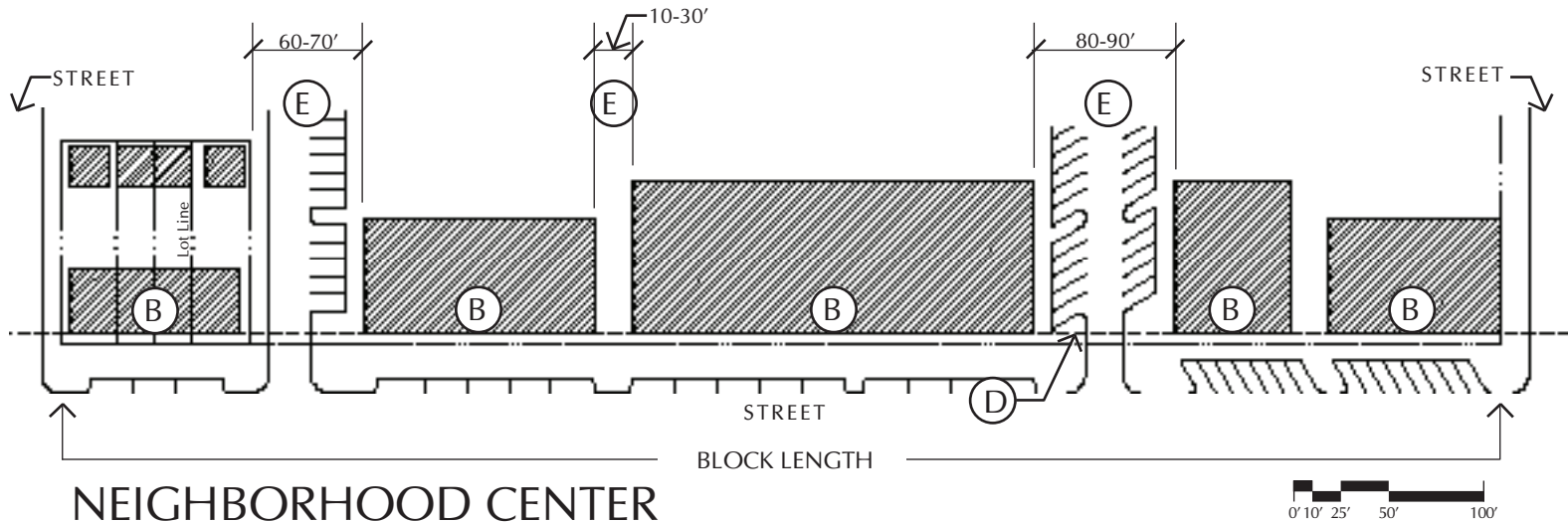
Maximum 75 feet; 76 - 90 feet with approval of Planning Director and 91 - 120 feet with approval of Town Council

G OTHER PROVISIONS:

- Open Space Types: Plazas or Squares
- Open Space Frequency: 1 Plaza or Square per 10 acres of developable area; evenly distributed and pedestrian-oriented (refer to Chapter IV Part 3, Definitions, for minimum and maximum sizes).
- Residential Lot Widths: 16 to 30 foot wide lots except where road alignment may necessitate a different width
- Preferred Maximum Block Length: 800 feet from circulation route to circulation route
- Cafe seating (tables, chairs, planters and rails) is encouraged in the Town Center, but shall occur outside the public right-of-way. Clear passage (minimally 5 feet) on public sidewalks shall be maintained.
- A single retail tenant or building is limited to a maximum of 100,000 square feet; 100,001 - 125,000 square feet with approval of Town Council. Also with approval of Town Council, 125,000 square feet may be exceeded if structured parking is provided and there is no increase in acreage devoted to parking.
- Off-street parking requirement for personal service establishment, retail store, and shopping center, general: minimum of 4.25 spaces per 1,000 square feet

III - DESIGN STANDARDS

1 - DESIGN ZONES



NEIGHBORHOOD CENTER

A MAIN STREET:

“Main streets” are typically composed of Collector and/or Street A circulation types; they meet or exceed frontage coverage and non-first floor space and vertical mixed use requirements described below. “Main streets” are consistently double-loaded except where natural features such as stream buffers or steep slopes (greater than 15%) result in a single-loaded street segment. These streets are destination points within the Alston area that foster a positive pedestrian experience; each neighborhood will have at least one contiguous “main street”.

A contiguous “main street” should be provided that is at least equivalent in length to either

- 75% of the total combined length of the required Collector and Street A segments as indicated in the Circulation Diagram (see Chapter III Part 2) but not less than 900 feet (the length of any Collector or Street A that must be single-loaded due to environmental or road geometry constraints counts 50% toward the required minimum length of the contiguous “main street”), or
- 1,200 feet.

Notes:

- No more than 10% of a “main street” may be single-loaded; clear hardship, or no practical alternative, must be demonstrated to include a single-loaded section. The length of any single-loaded portions of the “main street” counts 50% towards meeting the required minimum length.
- “Main street” lengths required to be 1,200 feet may be reduced to no less than 900 feet by Town Council.
- Up to 50% of a “main street” may be composed of a completely residential product.
- “Main streets” may be one continuous linear street or may be a collection of contiguous streets.
- If less than 50,000 square feet of commercial and/or office floor space is developed in one design zone area, then main street and non-first floor space requirements do not have to be met. (The remainder of the design standards still have to be met.)

B FRONTAGE COVERAGE:

Frontage is defined as the linear dimension adjacent to a street along which the front façade(s) of structure(s) are placed. Frontage coverage is defined as the ratio of the total building frontage lining a street block to the total length of the block.

Frontage coverage of 60% minimum of block length (sum of building lengths divided by block length) is required on “main streets”. Block length is measured between streets (not including alleys) or to a property line or stream buffer. Required end-of-block setbacks may be subtracted from the block length.

Notes:

- Frontage coverage is achieved with buildings. However, Plazas, Squares, and Greens, as specified for the Neighborhood Center zone, also count towards frontage coverage, as long as they are addressed with buildings on all sides whenever possible.

III - DESIGN STANDARDS

1 - DESIGN ZONES

- b) Area of Frontage Coverage should be contiguous and compact.
- c) Although Frontage Coverage is not required for all Circulation Types, views into large parking lots should be screened with buildings, walls, plantings, or similar.

C MIXED USE:

At least two different uses, mixed vertically or horizontally, are required along a main street. The floor space for one use may not exceed 80% of the total square footage along that main street.

D BUILD-TO LINE (FRONT SETBACK):

Five to fifteen feet from property line or from private street back of curb; twenty-five to thirty feet from property line along thoroughfares. Exceptions for drive-throughs may be granted by the Planning Director.

E BUILDING SEPARATIONS/SIDE SETBACKS:

Shall be within the dimensions as noted in the diagram above. When necessary to accommodate required and/or reasonably distributed parking and/or deal with other site constraints, greater building separations/side setbacks may be allowed as follows:

- a) For every 500 feet of "main street" length or other street length, there may be one side setback/building separation between 91 - 170 feet.
- b) Additional side setbacks/building separations between 91 - 170 feet may be approved by the Planning Director.
- c) A wall and/or landscaping will be required to fill in any building separation gaps.

F BUILDING HEIGHT:

Minimum 20 feet (or 1.5 to 2 story appearance)

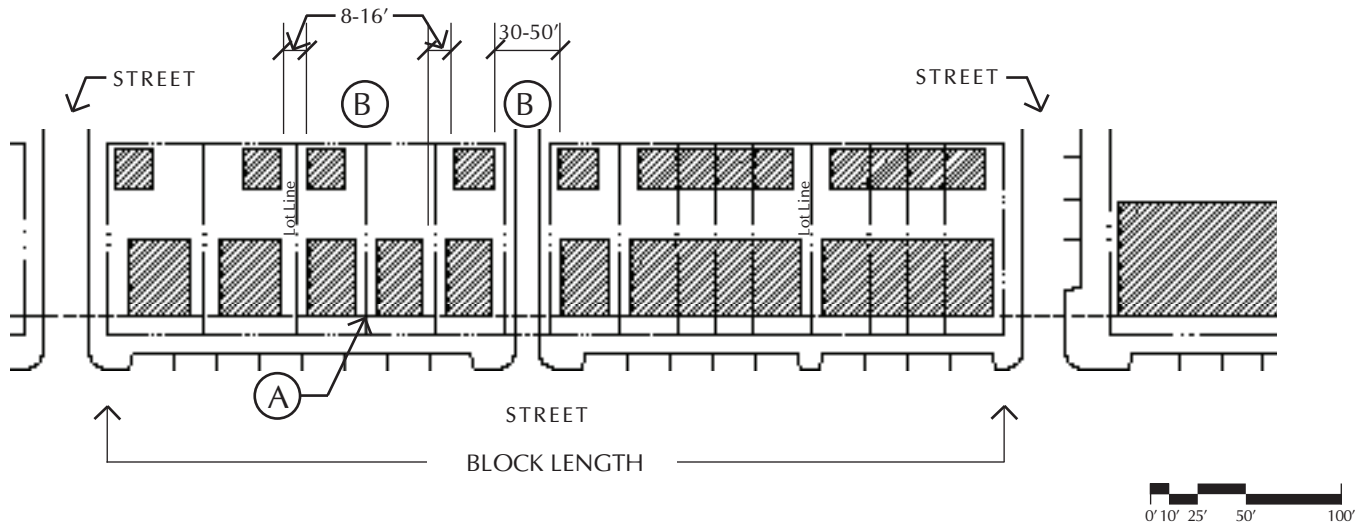
Maximum 75 feet; 76 - 90 feet with approval of Planning Director and 91 - 120 feet with approval of Town Council

G OTHER PROVISIONS

- Open Space Types: Plazas, Squares or Greens
- Open Space Frequency: 1 Plaza, Square or Green per 10 acres of developable area; evenly distributed and pedestrian-oriented (refer to Chapter IV Part 3, Definitions, for minimum and maximum sizes).
- Residential Lot Widths: 16 to 40 foot wide lots except where road alignment may necessitate a different width
- Preferred Maximum Block Length: 800 feet from circulation route to circulation route
- Cafe seating (tables, chairs, planters and rails) is encouraged in the Neighborhood Center, but shall occur outside the public right-of-way. Clear passage (minimally 5 feet) on public sidewalks shall be maintained.
- A single retail tenant or building is limited to a maximum of 70,000 square feet; 70,001 - 90,000 square feet with approval of Town Council. Also with approval of Town Council, 90,000 square feet may be exceeded if structured parking is provided and there is no increase in acreage devoted to parking.
- Off-street parking requirement for personal service establishment, retail store, and shopping center, general: minimum of 4.25 spaces per 1,000 square feet

III - DESIGN STANDARDS

1 - DESIGN ZONES



NEIGHBORHOOD GENERAL

(A) BUILD-TO-LINE (FRONT SETBACK):

Ten to twenty feet from property line or from private street back of curb; twenty-five to thirty feet from property line along thoroughfares.

(B) BUILDING SEPARATIONS/SIDE SETBACKS:

Shall be within the dimensions as noted in the diagram above.

(C) REAR SETBACKS:

Shall follow Transitional Residential District (TR) standards or 4 feet with an alley.

(D) BUILDING HEIGHT:

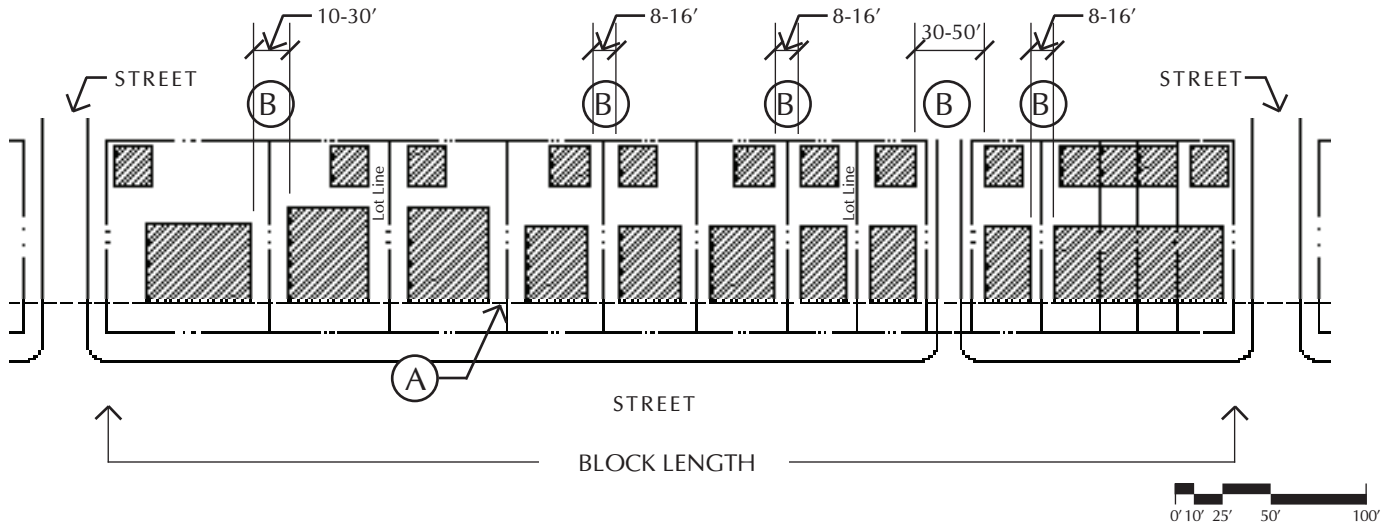
Maximum 45 feet

(E) OTHER PROVISIONS:

- Open Space Types: Greens or Parks
- Open Space Frequency: 1 Green or Park per 10 acres of developable area; evenly distributed and pedestrian-oriented (refer to Chapter IV Part 3, Definitions, for minimum and maximum sizes).
- Residential Lot Widths: 20 to 60 foot wide lots except where road alignment may necessitate a different width
- Preferred Maximum Block Length: 600 feet from circulation route to circulation route

III - DESIGN STANDARDS

1 - DESIGN ZONES



NEIGHBORHOOD EDGE

(A) BUILD-TO-LINE (FRONT SETBACK):

Fifteen to twenty-five feet from property line or from private street back of curb; twenty-five to thirty feet from property line along thoroughfares.

(B) BUILDING SEPARATIONS/SIDE SETBACKS:

Shall be within the dimensions as noted in the diagram above.

(C) REAR SETBACKS:

Shall follow Transitional Residential District (TR) standards or 4 feet with an alley.

(D) BUILDING HEIGHT:

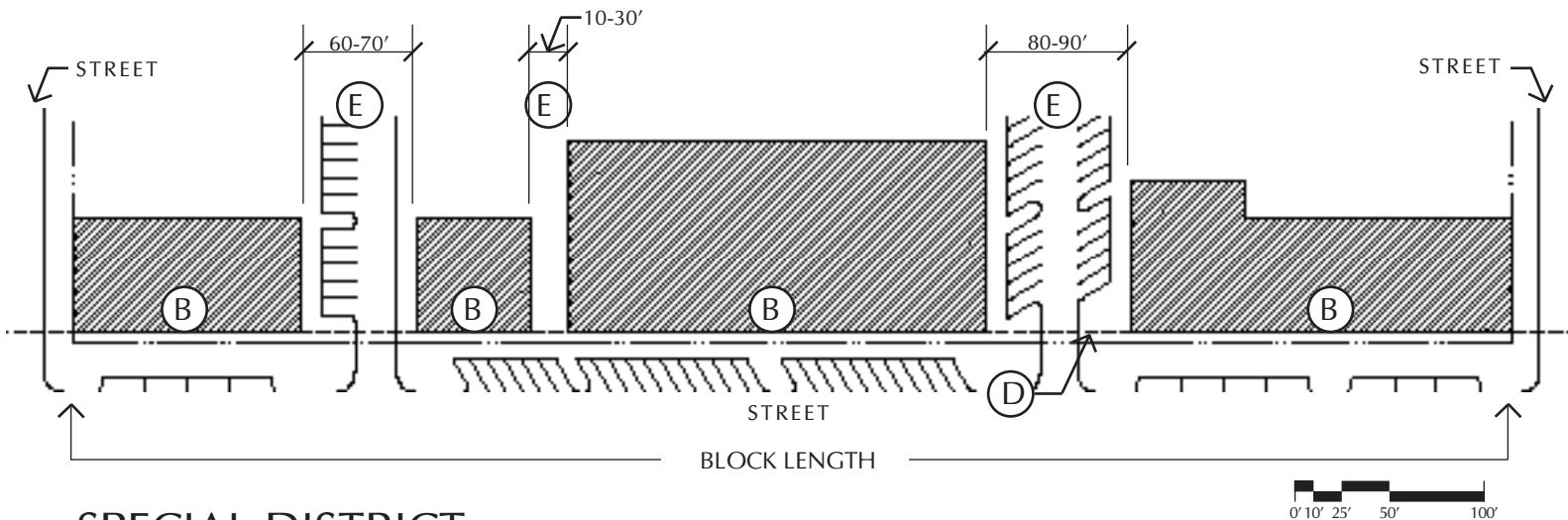
Maximum 45 feet

(E) OTHER PROVISIONS:

- Open Space Types: Greens or Parks
- Open Space Frequency: 1 Green or Park per 10 acres of developable area; evenly distributed and pedestrian-oriented (refer to Chapter IV Part 3, Definitions, for minimum and maximum sizes).
- Residential Lot Widths: 20 to 100 foot wide lots except where road alignment may necessitate a different width
- Preferred Maximum Block Length: 600 feet from circulation route to circulation route

III - DESIGN STANDARDS

1 - DESIGN ZONES



SPECIAL DISTRICT

A MAIN STREET:

“Main streets” are typically composed of Collector and/or Street A circulation types; they meet or exceed frontage coverage and non-first floor space and vertical mixed use requirements described below. “Main streets” are consistently double-loaded except where natural features such as stream buffers or steep slopes (greater than 15%) result in a single-loaded street segment. These streets are destination points within the Alston area that foster a positive pedestrian experience; each neighborhood will have at least one contiguous “main street”.

A contiguous “main street” should be provided that is at least equivalent in length to either

- 75% of the total combined length of the required Collector and Street A segments as indicated in the Circulation Diagram (see Chapter III Part 2) but not less than 1,800 feet (the length of any Collector or Street A that must be single-loaded due to environmental or road geometry constraints counts 50% toward the required minimum length of the contiguous “main street”), or
- 2,400 feet.

Notes:

- No more than 10% of a “main street” may be single-loaded; clear hardship, or no practical alternative, must be demonstrated to include a single-loaded section. The length of any single-loaded portions of the “main street” counts 50% towards meeting the required minimum length.
- “Main street” lengths required to be 2,400 feet may be reduced to no less than 1,800 feet by Town Council.
- Up to 50% of a “main street” may be composed of a completely residential product.
- “Main streets” may be one continuous linear street or may be a collection of contiguous streets.

B FRONTAGE COVERAGE:

Frontage is defined as the linear dimension adjacent to a street along which the front façade(s) of structure(s) are placed. Frontage coverage is defined as the ratio of the total building frontage lining a street block to the total length of the block.

Frontage coverage of 70% minimum of block length (sum of building lengths divided by block length) is required on “main streets”. Block length is measured between streets (not including alleys) or to a property line or stream buffer. Required end-of-block setbacks may be subtracted from the block length.

Notes:

- Frontage coverage is achieved with buildings. However, Plazas and Squares, as specified for the Special District zone, also count towards frontage coverage, as long as they are addressed with buildings on all sides whenever possible.
- Area of Frontage Coverage should be contiguous and compact.

III - DESIGN STANDARDS

1 - DESIGN ZONES

- c) Although Frontage Coverage is not required for all Circulation Types, views into large parking lots should be screened with buildings, walls, plantings, or similar.

C MIXED USE:

At least two different uses, mixed vertically or horizontally, are required along a main street. The floor space for one use may not exceed 80% of the total square footage along that main street.

D BUILD-TO-LINE (FRONT SETBACK):

Five to ten feet from property line or from private street back of curb; twenty-five to thirty feet from property line along thoroughfares. Exceptions for drive-throughs may be granted by the Planning Director.

E BUILDING SEPARATIONS/SIDE SETBACKS:

Shall be within the dimensions as noted in the diagram above. When necessary to accommodate required and/or reasonably distributed parking and/or deal with other site constraints, greater building separations/side setbacks may be allowed as follows:

- a) For every 1000 feet of "main street" length or other street length, there may be two side setback/building separations between 91 - 170 feet.
- b) Additional side setbacks/building separations between 91 - 170 feet may be approved by the Planning Director.
- c) A wall and/or landscaping will be required to fill in any building separation gaps.

F BUILDING HEIGHT:

Minimum 20 feet (or 1.5 to 2 story appearance)

Maximum 120 feet; 121 - 144 feet with approval of Planning Director and 145 - 200 feet with approval of Town Council

G OTHER PROVISIONS:

- Open Space Types: Plazas or Squares
- Open Space Frequency: 1 Plaza or Square per 10 acres of developable area; evenly distributed and pedestrian-oriented (refer to Chapter IV Part 3, Definitions, for minimum and maximum sizes).
- Residential Lot Widths: 16 to 30 foot wide lots except where road alignment may necessitate a different width
- Preferred Maximum Block Length: 800 feet from circulation route to circulation route
- Cafe seating (tables, chairs, planters and rails) is encouraged in the Special District, but shall occur outside the public right-of-way. Clear passage (minimally 5 feet) on public sidewalks shall be maintained.
- A single retail tenant or building is limited to a maximum of 180,000 square feet; 180,001 - 225,000 square feet with approval of Town Council. Also with approval of Town Council, 225,000 square feet may be exceeded if structured parking is provided and there is no increase in acreage devoted to parking.
- Off-Street parking requirement for personal service establishment, retail store, and shopping center, general: minimum of 4.25 spaces per 1,000 square feet